# PRELIMINARY SITE PLAN FOR: PELICAN GOLF CLUB PHASE III EXPANSION

SECTION 28, TOWNSHIP 29 SOUTH, RANGE15 EAST TOWN OF BELLEAIR, FLORIDA

Prepared For:

# PELICAN GOLF CLUB, LLC

11201 CORPORATE CIRCLE N, SUITE 120 ST. PETERSBURG, FLORIDA 33716

Prepared By:



**Civil & Environmental Engineers - Planners - Surveyors** 

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#### **PERMITS**

PERMIT TYPE	NUMBER	APPROVED
TOWN OF BELLEAIR PRELIMINARY SITE PLAN		
TOWN OF BELLEAIR FINAL DEVELOPMENT PLAN		
SWFWMD PERMIT MODIFICATION		
FDEP NPDES		
FDEP SEWER PERMIT		
FDEP WATER PERMIT		

#### REVISIONS

REVISION DATE	COMMENTS

## LEGAL DESCRIPTION

LOCATION MAP

MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 18, PAGE 52, OF THE PUBLIC RECORDS OF PINELLAS COUNTY,

LOT 4. LESS THE NORTHERLY 16.00 FEET. AND ALL OF LOT 5. BLOCK 32. A SUBDIVISION OF BELLEAIR ESTATES EAST OF INDIAN ROCKS ROAD, ACCORDING TO THE MAP OR PLAT THEREOF, AS RECORDED IN PLAT BOOK 18, PAGE 52, OF THE PUBLIC RECORDS OF PINELLAS COUNTY, FLORIDA.

PARCEL 4: LOTS 32, 33 AND 34, BLOCK 32, A SUBDIVISION OF BELLEAIR ESTATES EAST OF INDIAN ROCKS ROAD, ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 18, PAGE 52, OF THE PUBLIC RECORDS OF PINELLAS

TOGETHER WITH:

THAT PORTION OF ALTHEA ROAD (GOLF VIEW DRIVE - FIELD) LYING NORTHERLY OF AND ADJOINING TO LOTS 1 AND 34, BLOCK 32, TOGETHER WITH THAT PORTION OF GOLF VIEW DRIVE LYING EASTERLY OF AND ADJOINING TO LOTS 32, 33 AND 34, BLOCK 32, ALL LYING WITHIN A SUBDIVISION OF BELLEAIR ESTATES EAST OF INDIAN ROCKS ROAD, ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 18, PAGES 52 THROUGH 57, OF THE PUBLIC RECORDS OF PINELLAS COUNTY, FLORIDA, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID LOT 1, BLOCK 32; THENCE N25'17'43"E, ALONG THE SOUTHEASTERLY RIGHT-OF-WAY OF INDIAN ROCKS ROAD, A DISTANCE OF 60 FEET TO A POINT ON THE WESTERLY BOUNDARY OF PELICAN GOLF COURSE, ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN PLAT BOOK 53, PAGE 45, OF THE PUBLIC RECORDS OF PINELLAS COUNTY, FLORIDA; THENCE ALONG SAID WESTERLY BOUNDARY OF PELICAN GOLF COURSE FOR THE FOLLOWING TWO (2) COURSES AND DISTANCES: (1) S64\*42'17"E, A DISTANCE OF 310 FEET, (2) S25'17'43"W, A DISTANCE OF 208 FEET; THENCE N64'42'17"W, ALONG THE EASTERLY EXTENSION OF THE SOUTHERLY BOUNDARY OF SAID LOT 32, BLOCK 32, A DISTANCE OF 60 FEET TO THE SOUTHEAST CORNER OF SAID LOT 32, BLOCK 32; THENCE N2517'43"E, ALONG THE EASTERLY BOUNDARY OF SAID BLOCK 32, A DISTANCE OF 148 FEET TO THE NORTHEAST CORNER OF SAID LOT 34, BLOCK 32; THENCE N64\*42'17"W, ALONG THE NORTHERLY BOUNDARY OF SAID BLOCK 32, A DISTANCE OF 250 FEET TO THE POINT OF BEGINNING.

COMBINED PARCELS AND RIGHT-OF-WAY CONTAIN 76,759 SQUARE FEET OR 1.76 ACRES, MORE OR LESS.

ALL ELEVATIONS REFER TO NAVD 88 DATUM UNLESS OTHERWISE NOTED

# Call before you dig.

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TREE BARRICADES & EROSION CONTROL MEASURES NOTE REQUIRED TREE BARRICADES AND EROSION CONTROL MEASURES MUST REMAIN INTACT THROUGHOUT CONSTRUCTION. ENCROACHMENT INTO OR FAILURE TO MAINTAIN THESE BARRICADES WILL RESULT IN ENFORCEMENT ACTION WHICH MAY INCLUDE CITATIONS AND/OR PERMIT REVOCATION AS PROVIDED BY THE TOWN OF BELLEAIR LAND DEVELOPMENT CODE.

NOTE TO CONTRACTOR/OWNER ONSTRUCTION IS NOT AUTHORIZED UNTIL PLANS HAVE BEEN ISSUED BY LANDON. MOREE & ASSOCIATES. INC. STAMPED "ISSUED FOR CONSTRUCTION". ALL DESIGN AND CONSTRUCTION MUST CONFORM TO THE MINIMUM STANDARDS SET FORTH IN THE TOWN OF BELLEAIR LAND DEVELOPMENT, ZONING, AND/OR RELATED ORDINANCES AND SPECIFICATIONS AS APPLICABLE.

### PLAN INDEX

- 1. COVER SHEET
- 2. AERIAL PHASE PLAN

#### PHASE III - EXPANSION AREA

- 3. EXISTING BOUNDARY SURVEY
- DEMOLITION/EROSION CONTROL PLAN
- 5. PRELIMINARY SITE PLAN
- PRELIMINARY DRAINAGE DESIGN

#### PHASE III - MAINTENANCE AREA

- 7. DEMOLITION/EROSION CONTROL PLAN
- 8. PRELIMINARY SITE PLAN
- 9. PRELIMINARY DRAINAGE DESIGN
- CONSTRUCTION NOTES & DETAILS
- 11. DETAILS

## SITE DATA

### EXISTING PELICAN GOLF CLUB

ROSS SITE AREA: 136.0 ACRES±

ZONING: GC (GOLF COURSE)

#### PHASE III MAINTENANCE AREA 25,201 SF (0.58 ACRES)

**EXISTING CONDITIONS:** 

TOTAL IMPERVIOUS AREA: 11,424 SF (45.3%)

BUILDING AREA: 3,470 SF (13.8%) VEHICULAR USE AREA: 4,640 SF (18.4%)

#### TOTAL IMPERVIOUS AREA: 20,198 SF (80.1%) BUILDING AREA: 6,770 SF (26.9%)

VEHICULAR USE AREA: 9,786 SF (38.8%) TOTAL OPEN SPACE: 5.003 SF (19.9%) TOTAL OPEN SPACE: 13,777 SF (54.7%)

#### PHASE III EXPANSION AREA (TO BE ADDED TO EXISTING GOLF COURSE VIA REPLAT) PARCEL ID #'s: 28-29-15-06732-032-0010, 28-29-15-06732-032-0021, 28-29-15-06732-032-0030,

28-29-15-06732-032-0040, 28-29-15-06732-032-0050 & 28-29-15-06732-032-0320 GROSS SITE AREA: 76,759 SF (1.76 ACRES) MAXIMUM BUILDING HEIGHT:

EXISTING LOTS: 49,279 SF

VACATED ROW: 27,480 SF

PROPOSED ZONING: GC (GOLF COURSE)

PROPOSED FLU: REC OPEN SPACE

**EXISTING CONDITIONS:** 

TOTAL IMPERVIOUS AREA: 12,888 SF (16.8%) BUILDING AREA: 0 SF (0.0%)

VEHICULAR USE AREA: 12,888 SF (16.8%) TOTAL OPEN SPACE: 63,871 SF (83.2%)

PROPOSED MAX. BUILDING HEIGHT: 28 FT. **BUILDING SETBACKS:** 

#### FRONT - 25 FT.

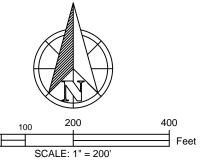
 SIDE - 25 FT. REAR - 25 FT.

PROPOSED CONDITIONS:

TOTAL IMPERVIOUS AREA: 13,183 SF (17.2%) BUILDING AREA: 6,554 SF (8.5%) VEHICULAR (GOLF CART) USE AREA: 4,441 (5.8%) TOTAL OPEN SPACE: 63,576 SF (82.8%)

WATER - TOWN OF BELLEAIR, SEWER - PINELLAS COUNTY POWER - DUKE ENERGY, TELEPHONE - VERIZON

THIS PROPERTY LIES WITHIN FLOOD ZONE "X" IN ACCORDANCE WITH THE FLOOD INSURANCE RATE MAP OF THE TOWN OF BELLEAIR, PINELLAS COUNTY, COMMUNITY NUMBER 125088 (MAP NUMBER 12103C-0116-H), MAP DATED MAY 17, 2005.



PHASING DETAILS

PHASE I - GOLF COURSE RENOVATION UNDER CONSTRUCTION

PHASE II - NEW CLUBHOUSE & EVENT LAWN UNDER CONSTRUCTION

## PHASE III - PROPOSED EXPANSION AREA AMENITIES TWO (2) 3,800 SF 2-STORY COTTAGES

- 2,000 SF GOLF LEARNING CENTER
- GOLF CART PATH EXTENSION LEADING TO NEW AMENITIES ALONG WITH 29 GOLF CART PARKING SPACES
- TWO (2) PUTTING GREENS
- RECONFIGURATION OF THE EXISTING PUTTING GREEN
- RELOCATION OF THE EXISTING DRIVING RANGE TEE BOX ALONG WITH THE EXPANSION OF THE EXISTING DRIVING RANGE

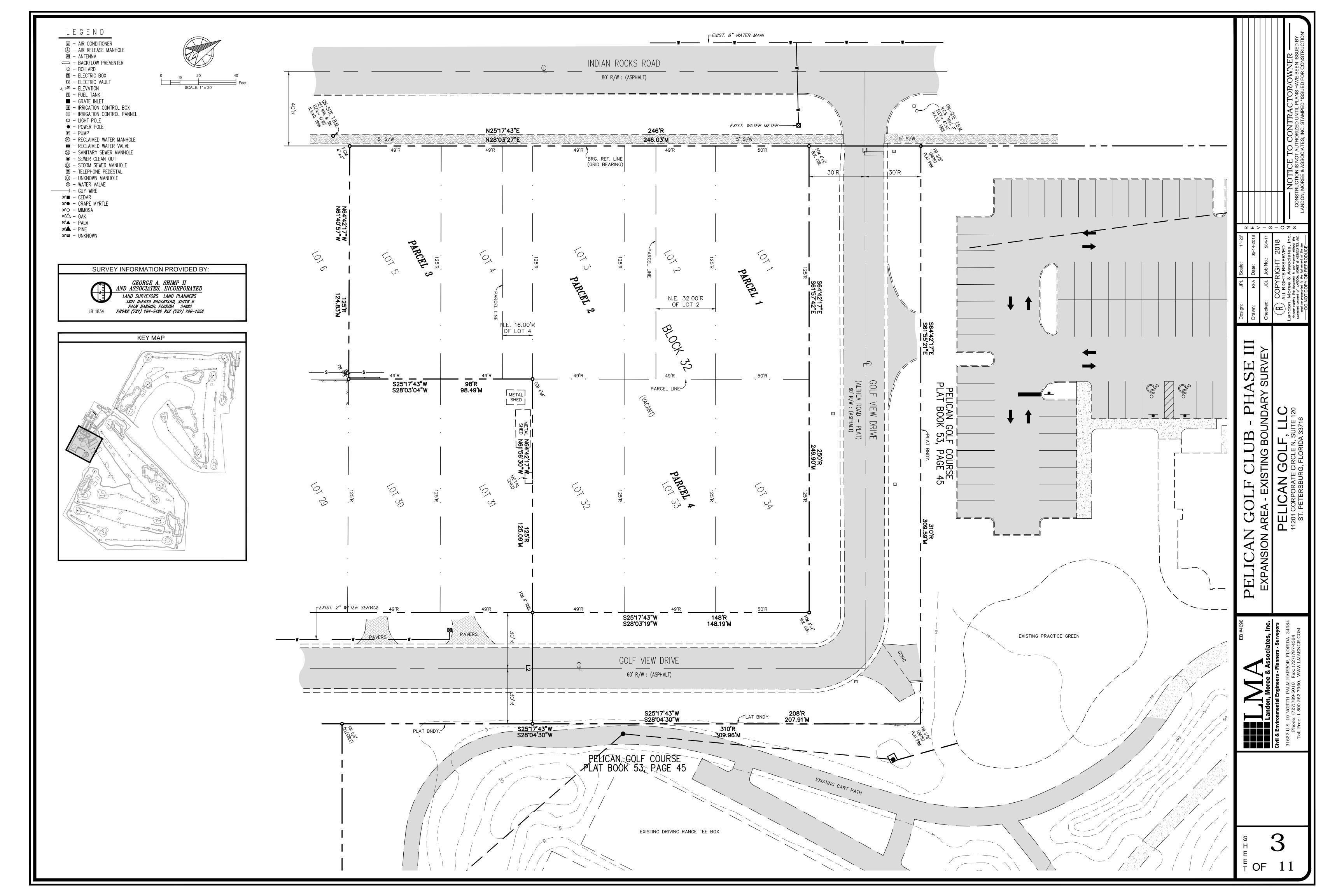
NOTE: THE PARCELS AND VACATED GOLF VIEW ROAD RIGHT-OF-WAY THAT COMPRISE THE PHASE II EXPANSION AREA ARE TO BE COMBINED WITH THE EXISTING GOLF COURSE PARCEL VIA REPLAT).

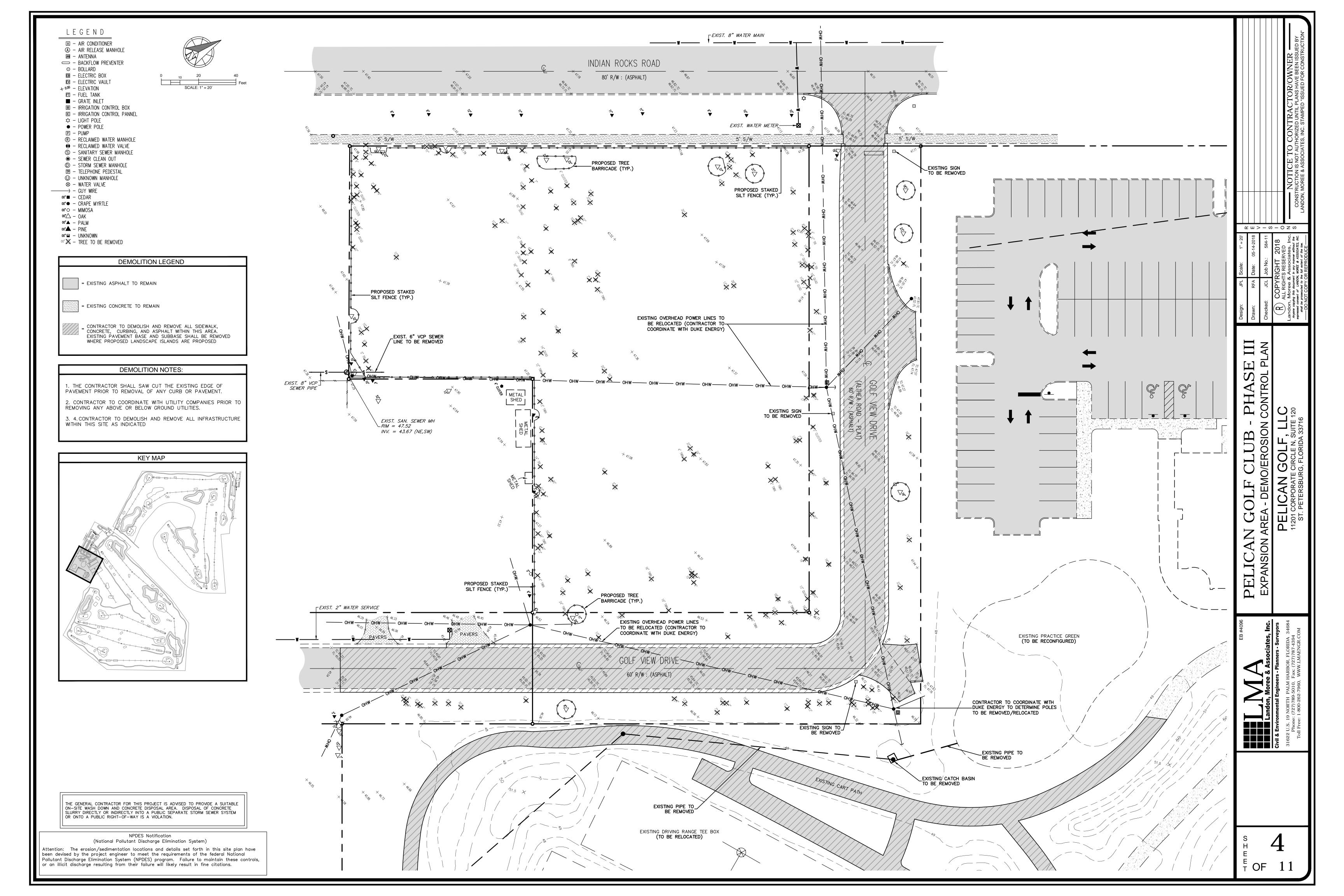
- PHASE III PROPOSED MAINTENANCE AREA ADDITION

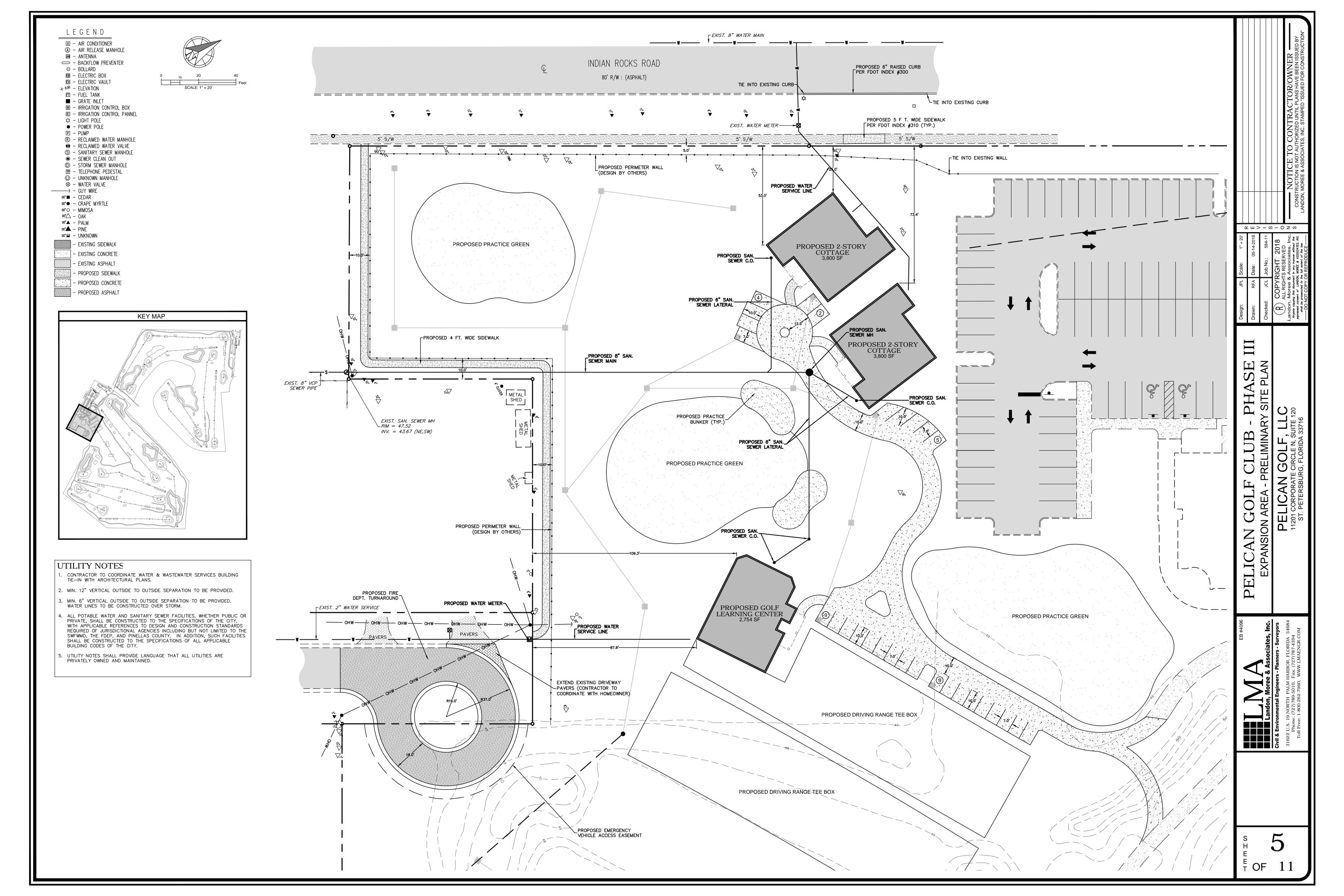
  3,300 SF METAL BUILDING FOR STORAGE/MAINTENANCE
- 12 SPACE PARKING LOT & DRIVEWAY EXPANSION
- 10'x22' CONCRETE APRON FOR FUEL TANK RELOCATION

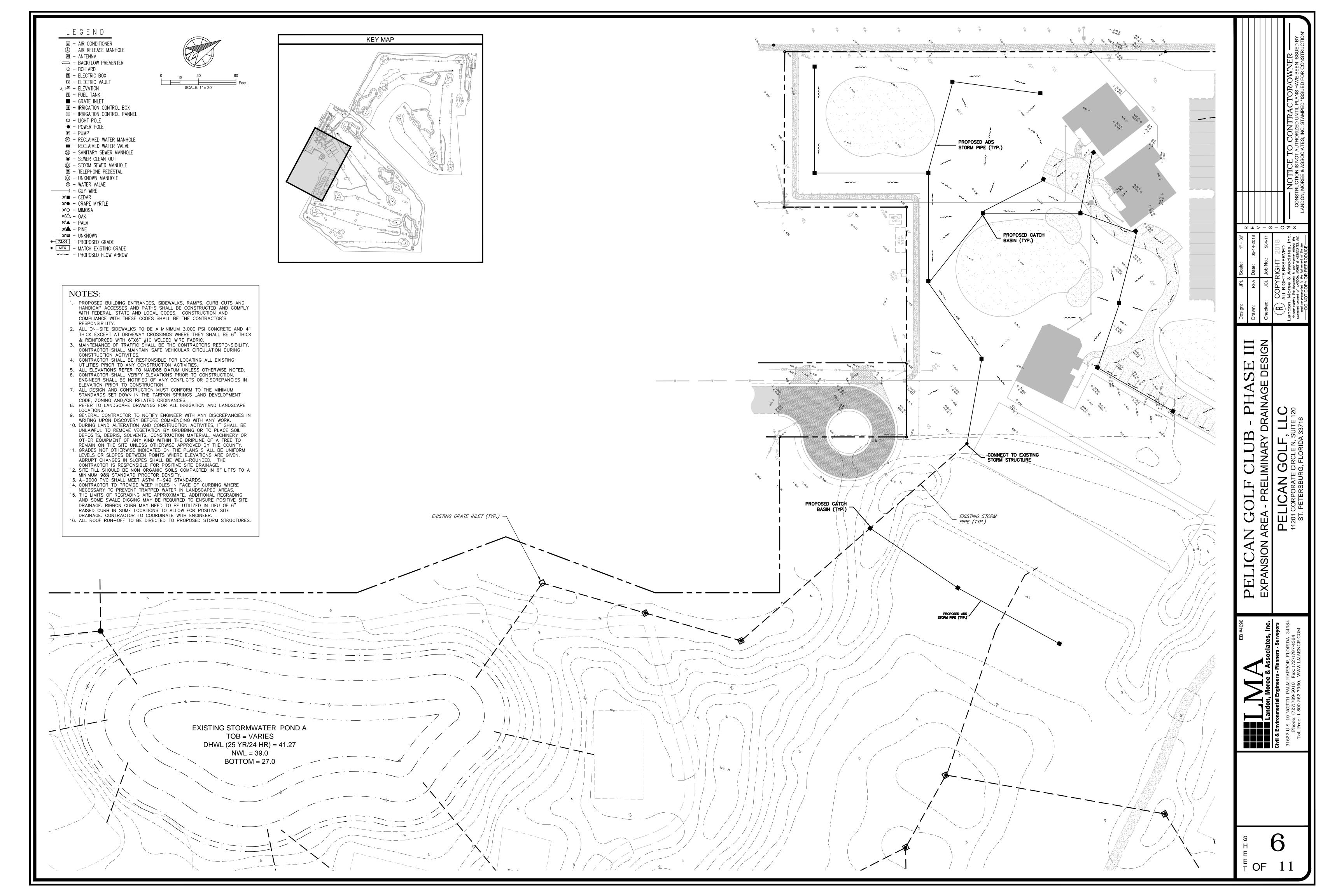
NOTE: A SHARED USE AGREEMENT IS TO BE EXECUTED BETWEEN PELICAN GOLF CLUB, LLC AND THE TOWN OF BELLEAIR FOR THE SHARED USE OF THE EXISTING MAINTENANCE AREA.

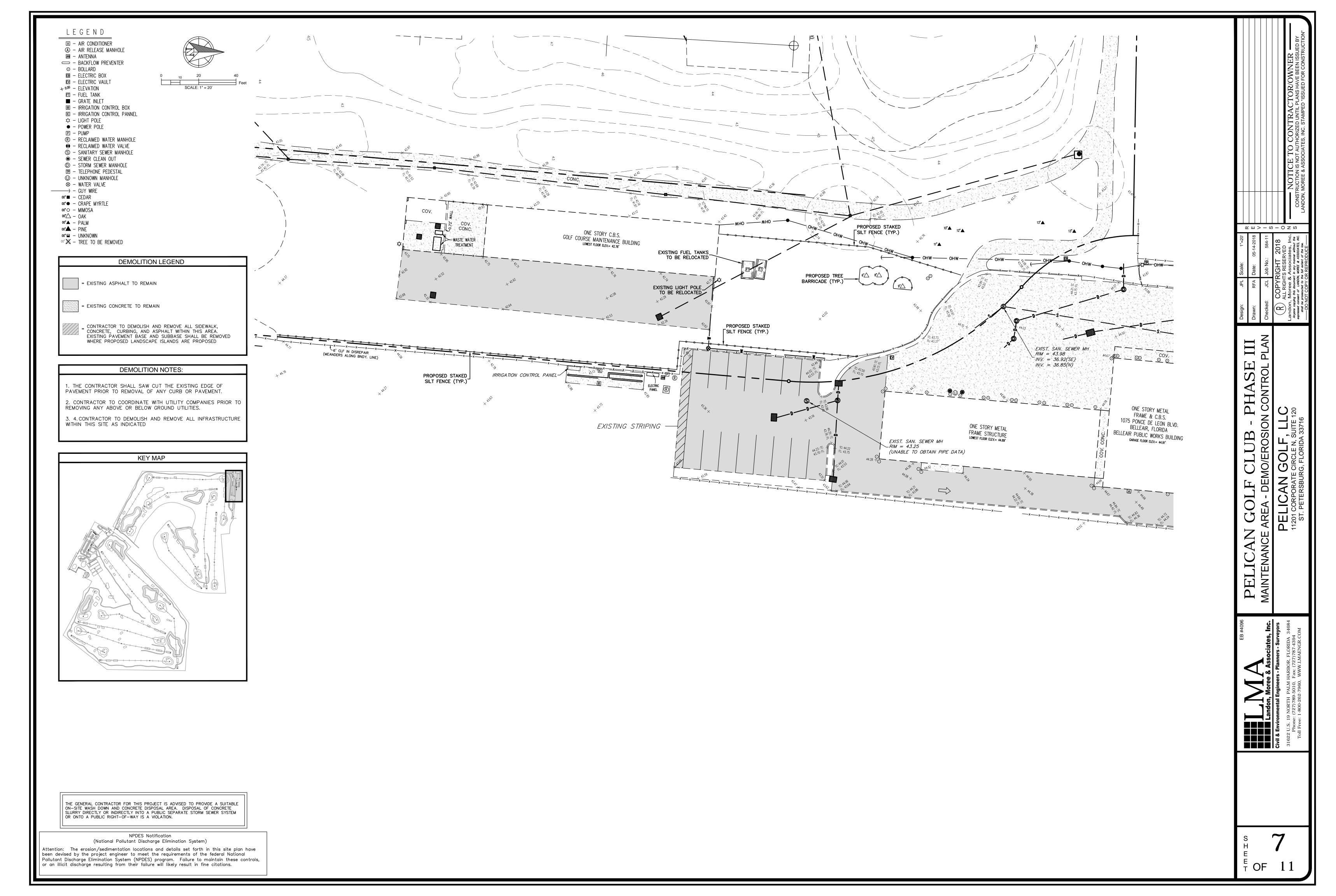


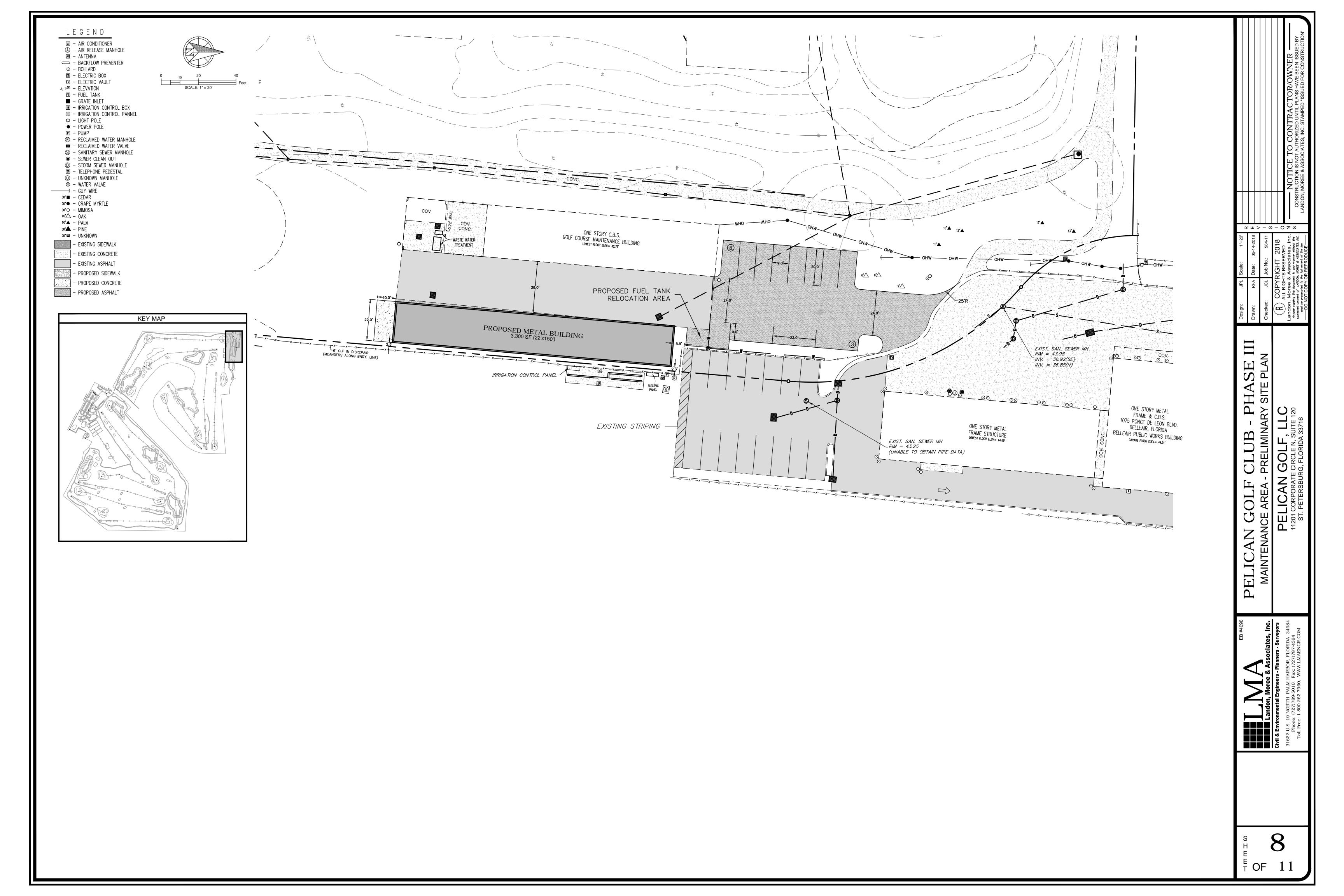


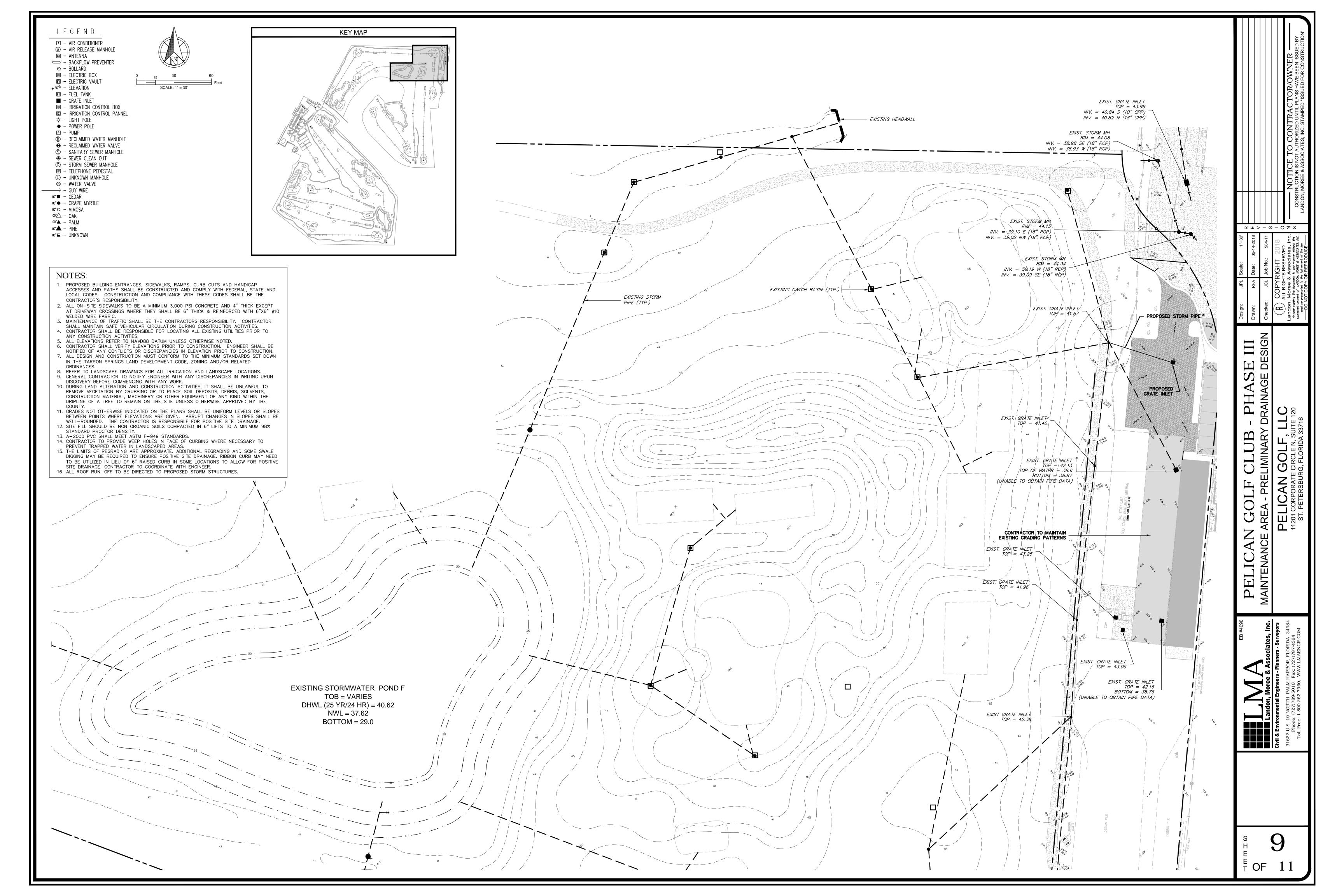












#### CONSTRUCTION TECHNICAL SPECIFICATIONS

1. All construction shall conform to the minimum standards set down in the Town of Belleair land development code, zoning, and/or related ordinances as applicable.

2. Prior to construction commencement, the Contractor shall obtain from the Engineer and/or Developer copies of all pertinent permits and approvals related to this project. It is the contractor's responsibility to properly post all permits on—site and to assure all construction activities are in compliance with the conditions of all permits and approvals

3. The technical specifications for construction of this project are indicated below. The contractor shall obtain a copy of said specifications and fully familiarize himself with the contents of these specifications. In the event that a discrepancy occurs between these plans and the specifications, the specifications shall take precedence, unless otherwise directed by the engineer in writing. The contractor shall keep a copy of the specifications on the job site at all times. Town of Belleair — Standard Technical Specifications (latest edition).

#### CONSTRUCTION NOTES

revocation.

All elevations are based on NAVD 88 datum. The contractor shall obtain a copy of the survey to verify this datum and also for determining Benchmark locations and elevations.

All existing information as provided by survey data prepared by George A. Shimp II & Associates,

Any discrepancies between the engineering plans, architectural plans, or Town of Belleair Codes shall be brought to the engineer's attention prior to construction.

All practicable and necessary effort shall be taken during construction to control and prevent erosion and transport of sediment to surface drains and to lakes and also to conform with permit

Plans are not valid for construction unless signed and sealed by a Professional Engineer

Required tree barricades must remain intact throughout construction. Encroachment into or failure to maintain tree barricades will result in enforcement action which may include citations and/or permit

8. Silt fence to be installed around all areas of proposed construction.

The contractor shall adhere to the Minimum Testing Frequency requirements of the agencies having

10. The contractor shall coordinate with the Geotechnical Engineer for any testing that may be required including, but not limited to, materials testing, asphalt, base and sub-base testing, concrete testing, building pad testing, and utility trench testing. Copies of these tests shall be given to the Engineer for review. A final Signed and Sealed report containing all testing shall be given to the Engineer after all

11. The contractor shall coordinate with the agencies/inspectors having jurisdiction over the project to determine what inspections will be required throughout the project. The contractor shall coordinate required inspections with the Developer and Engineer.

12. The contractor shall saw cut the existing edge of pavement prior to the removal of any curb or payement and also prior to payement tie-in.

13. Special care is to be taken so trees remain unharmed during construction.

14. Contractor shall furnish Landon, Moree & Associates, Inc. with one complete set of As-Built plans

16. Safe pedestrian traffic is to be maintained at all times.

signed and sealed by a surveyor upon completion of construction.

17. No stockpiling of material in roadway or on sidewalk. All dirt and debris shall be removed from iob site daily. Roads and sidewalk to be swept daily as part of daily clean up.

18. All subsurface construction shall comply with the "Trench Safety Act". It shall be the Contractor's responsibility to insure that all construction is in compliance with the Occupational Safety and Health Administration (OSHA) Regulations.

19. The soil and sediment control devises shall be installed prior to construction, maintained throughout construction, and until the site is permanently stabilized.

#### 20. All sodded slopes over 4:1, shall be installed with sod pegs.

Contractors shall test slopes steeper than 4:1 for stability. If it is determined that slope will need to be stabilized beyond sod secured with pegs, the contractor shall submit a stabilization plan utilizing geoweb or equivalent submitted to engineer for approval. Material shall be installed per manufacturer's

22. All sidewalks to be a minimum 3000 psi concrete and 4" thick except at driveway crossings where they shall be 6" reinforced w/ 6"x6" No. 10 w.w.f. Place expansion joints where 4" and 6" concrete

#### 23. Minimum pavement slope shall be 0.5%. Any areas of ponding shall be repaired at the Contractor's expense.

Disturbed area within the right-of-way will be compacted to 100% of maximum density and sodded. Type of sod used to restore the right-of-way shall be coordinated with the right-of-way owner.

25. Signs, pavement markings and barricades shall be accordance with the US Dept. of Fransportation's "Manual on Uniform Traffic Control Devices" and the Florida Dept. of Transportation's Roadways and Traffic Design Standards" Indexes 600 through 670 (latest edition).

26. All right-of-way installations shall comply with the State of Florida Utilities Accommodations Guide. 27. The location & elevation of existing utilities are approximate as shown and it is the contractor's responsibility to determine the exact location and elevation of the utilities prior to construction in their vicinity. Prior to utility construction, Engineer shall be notified of any discrepancies in utility elevations or locations. Deviations from the approved plan without the Engineer's approval will be subject to relocation

28. The Contractor shall be responsible for verifying the location of all utilities whether or not they are indicated on these plans and shall notify all utility owners a minimum of 48 hours in advance of construction commencement.

29. All underground utilities must be installed before base & surface course are constructed.

30. All pipe lengths are plus or minus and are measured to center of structures.

31. Conflicts between water, storm and underdrain systems to be resolved by adjusting water lines as

32. The contractor shall use extreme caution when connecting proposed storm sewer, sanitary sewer and water mains to existing mains or structures. Any damages caused shall be the contractor's responsibility.

33. Property owner shall maintain water and/or sewer lines or facilities on—site.

34. The contractor shall remove all abandoned utility lines, pipes, structures and other subterranean objects to a minimum depth of four (4) feet below grade, or as otherwise directed by the engineer.

35. All drainage structures must be constructed per Florida Department of Transportation Road Design Standards. All drainage structures must have a minimum interior floor space area of 10 square feet (except underdrain inspection boxes).

36. All backfill over any pipe (storm sewer, water line, sanitary sewer) that is to be installed under roadways or within the embankment, etc. of the roadway is to be compacted per Florida Department of Transportation Specifications.

All waterlines 4" and larger shall be AWWA C-900, DR 18. All waterlines less than 4" shall be ASTM D2241-05, SDR 21. Water lines shall be blue in color.

38. All sanitary sewer manhole rim elevations to match asphalt pavement.

39. Contractor to verify the elevation of existing pavement and structures before beginning any construction activity. Notify Engineer of any discrepancies in elevation.

40. All sanitary sewer mains, laterals and force mains shall have a minimum of 36 inches of cover. 41. All gravity sanitary sewer pipe shall be PVC sewer pipe conforming to ASTM D3034 SDR-26. PVC sewer pipe conforming to ASTM D3034 SDR-35 may be used only where specifically called for in the plans. Sanitary sewer pipe shall be green in color.

42. All sanitary sewer force mains 4" and larger shall be AWWA C-900, DR 18. All force mains less than 4" shall be ASTM D2241-05, SDR 21. Sanitary sewer force mains shall be green in color.

43. Points of connection for the sanitary sewer lines are to be coordinated with the building plumbing plans. Sanitary service connection locations shown on these plans are approximate.

44. If dewatering is conducted during construction, the contractor is responsible for the design, installation, operation, and subsequent removal of dewatering systems and their safety and conformity with local codes and regulations.

#### TOWN OF BELLEAIR ON-SITE CONSTRUCTION NOTES

The contractor shall provide a suitable on-site wash down area and concrete disposal area. Disposal of concrete slurry directly or indirectly into public drainage systems is a violation of the "illicit discharge" provision of the Tarpon Springs land development code.

Any exotic species, i.e. Brazilian pepper (schinus terebinthifolius), Punk Trees (melaleuca quinquenervia), and Chinese Tallow (sapium sebiferum), must be removed as a condition of site development. Where necessary due to their proximity to protected plant material, hand removal will be required. Should this removal be to a degree that a potential for erosion is created, the area must be restabilized with suitable material.

If temporary dewatering activities are needed during construction the water shall be treated prior to discharge. Treatment methods may include, for example, turbid water being pumped into grassed swales or appropriate upland vegetative areas, sediment basins, or confined by appropriate enclosure such as turbidity barriers or low berms, and kept confined until turbidity levels meet state water quality standards. Water can be transported through the site by the use of internal swales or by pumps and pipes. The contractor shall take all practical measures necessary in order to discharge treated water to the on-site detention pond.

4. All proposed work must comply with FDOT Index No. 700 (Latest Edition).

5. Compaction for pipe backfill shall comply with AASHTO T-99 (100%).

construction, and until the site is permanently stabilized.

The Permittee's attention is directed to the provisions of the Trench Safety Act (Florida Statutes, Section 553.60 et. Seq.) and the Occupational Safety and Health Administration Excavation Safety Standards (29 C.F.R. Section 1926.650, Subpart B) which shall apply to construction, operation and maintenance pursuant to this permit.

#### STORMWATER POLLUTION PREVENTION NOTES

The contractor shall at a minimum implement the stormwater pollution prevention measures as shown in this plan set. These measures may include, but not limited to, the construction of retention/detention ponds, control structures, tree barricades, and silt fences.

. In addition to the measures shown in this plan set, the contractor shall take all necessary and practical measures in order to reduce erosion and sedimentation to both on-site and off-site surface waters, lakes, and wetlands. These measures may include, but not limited to, geohay at stormwater inlets, floating turbidity barriers, geohay dikes, stabilization of steeps grades with sodding or seeding/mulching, site entrance/exit control stabilization with gravel, temporary sediment basins and berms.

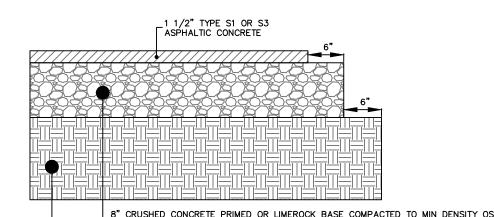
3. In addition to the measures shown in this plan set, the contractor shall take all necessary and practical measures in order to reduce airborne transportation of sediments. These measures may include, but not

The contractor shall at a minimum inspect the above referenced measures at least once a week or within 24 hours after 0.25 inches of rainfall. An inspection report shall be completed for each inspection. Any permit violation shall be noted and corrective measures taken shall be noted. This

report shall be given to Landon, Moree & Assoc., Inc. after construction completion. The cost of implementing the above measures are to be included within the contractors contract with

6. All practicable and necessary effort shall be taken during construction to control and prevent erosion and transport of sediment to surface drains and to lakes and also to conform with permit conditions.

7. Silt fence to be installed around all areas of proposed construction. (FDOT Index No. 102 Type III) 8. The soil and sediment control devises shall be installed prior to construction, maintained throughout

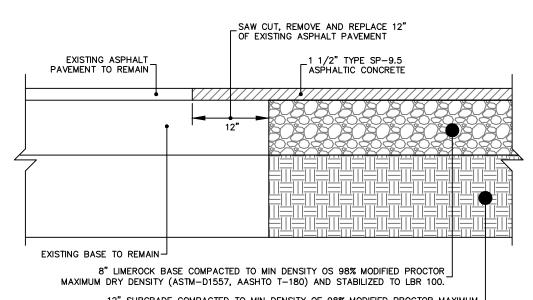


-98% MODIFIED PROCTOR MAXIMUM DRY DENSITY (ASTM-D1557, AASHTO T-180) AND 12" SUBGRADE COMPACTED TO MIN DENSITY OF 98% MODIFIED PROCTOR MAXIMUM DRY DENSITY (ASTM D-1557, AASHTO T-180) AND STABILIZED TO MIN LBR 40.

NOTE: PAVING SECTION SHALL BE CONSTRUCTED IN ACCORDANCE WITH TH LATEST FDITION OF THE FLORIDA DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION."

NOTE: THIS PAVING SECTION IS FOR WORK ON PRIVATE PROPERTY ONLY. FOR WORK WITHIN TARPON SPRINGS R/W, INCLUDING DRIVEWAY, REFER TO TARPON

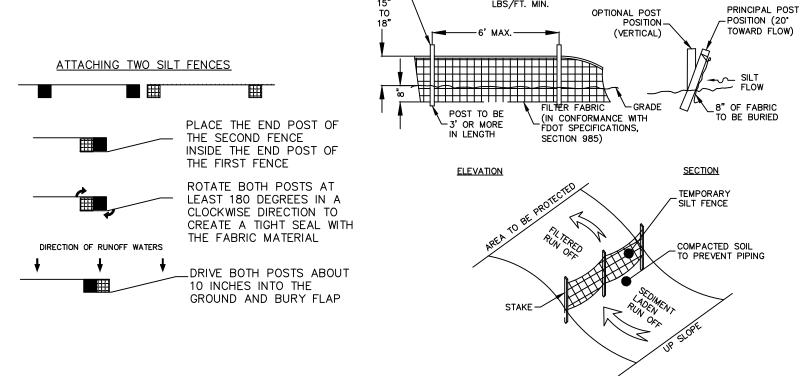
TYPICAL PAVEMENT SECTION



12" SUBGRADE COMPACTED TO MIN DENSITY OF 98% MODIFIED PROCTOR MAXIMUM\_ DRY DENSITY (ASTM D-1557, AASHTO T-180) AND STABILIZED TO MIN LBR 40.

PAVEMENT CONNECTION DETAIL

N.T.S.



NOTES:

1. THE FOLLOWING LIST REPRESENTS A BASIC EROSION AND SEDIMENT CONTROL PROGRAM WHICH IS TO BE IMPLEMENTED TO HELP PREVENT OFF SITE SEDIMENTATION DURING AND AFTER CONSTRUCTION OF THE PROJECT. 2. TEMPORARY EROSION CONTROL TO BE UTILIZED DURING CONSTRUCTION AT AREAS DESIGNATED BY THE ENGINEER OR AREAS ON SITE WHERE UNSTABILIZED GRADES MAY CAUSE EROSION PROBLEMS. EROSION CONTROL MAY BE REMOVED AFTER UPSLOPE AREA HAS BEEN STABILIZED BY SOD, OR COMPACTED AS

3. PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE INSTALLED AT THE EARLIEST PRACTICAL TIME CONSISTENT WITH GOOD CONSTRUCTION PRACTICES. ONE OF THE FIRST CONSTRUCTION ACTIVITIES SHOULD BE THE PLACEMENT OF PERMANENT AND TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES AROUND THE PERIMETER OF THE PROJECT OR THE INITIAL WORK AREA TO PROTECT THE PROJECT, ADJACENT PROPERTIES AND WATER

4. TEMPORARY FROSION AND SEDIMENT CONTROL MEASURES SHALL BE COORDINATED WITH PERMANENT MEASURES TO ASSURE ECONOMICAL, EFFECTIVE AND CONTINUOUS CONTROL THROUGHOUT THE CONSTRUCTION PHASE. TEMPORARY MEASURES SHALL NOT BE CONSTRUCTED FOR EXPEDIENCY IN LIEU OF PERMANENT MEASURES.

5. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE ADEQUATELY MAINTAINED TO PERFORM THEIR INTENDED FUNCTION DURING CONSTRUCTION OF THE PROJECT.

6. NECESSARY REPAIRS TO BARRIERS OR REPLACEMENT OF BARRIERS SHALL BE 7. SEDIMENT DEPOSITS SHOULD BE REMOVED AFTER EACH RAINFALL. THEY MUST BE REMOVED WHEN THE LEVEL OF DEPOSITION REACHES APPROXIMATELY ONE

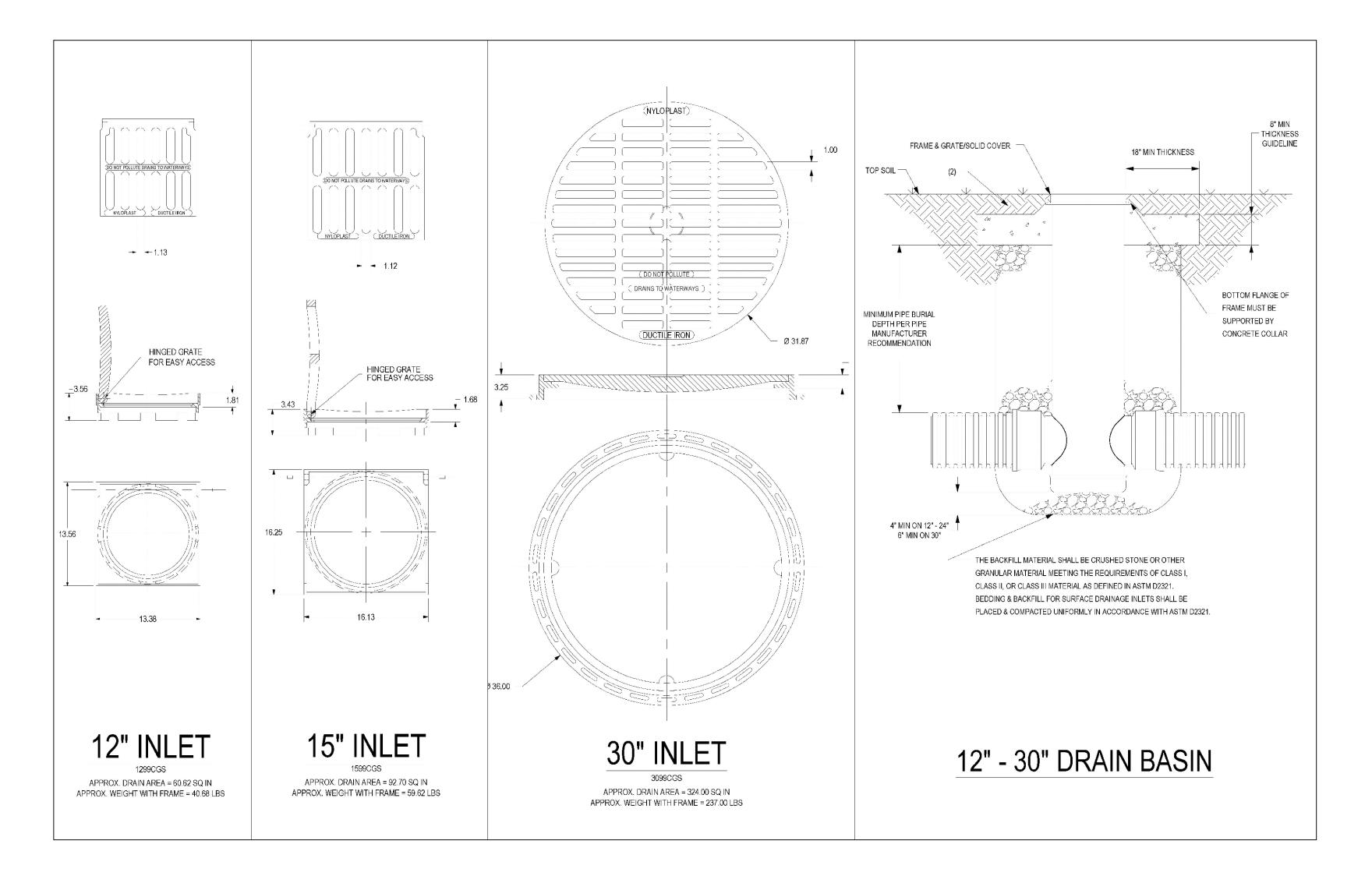
8. MATERIAL FROM SEDIMENT TRAPS SHALL NOT BE STOCKPILED OR DISPOSED OF IN A MANNER WHICH MAKES THEM READILY SUSCEPTIBLE TO BEING WASHED INTO ANY WATERCOURSE BY RUNOFF OR HIGH WATER.

9. ANY SEDIMENT DEPOSITS REMAINING IN PLACE AFTER THE BARRIERS ARE NO LONGER REQUIRED SHALL BE DRESSED TO CONFORM TO THE EXISTING GRADE,

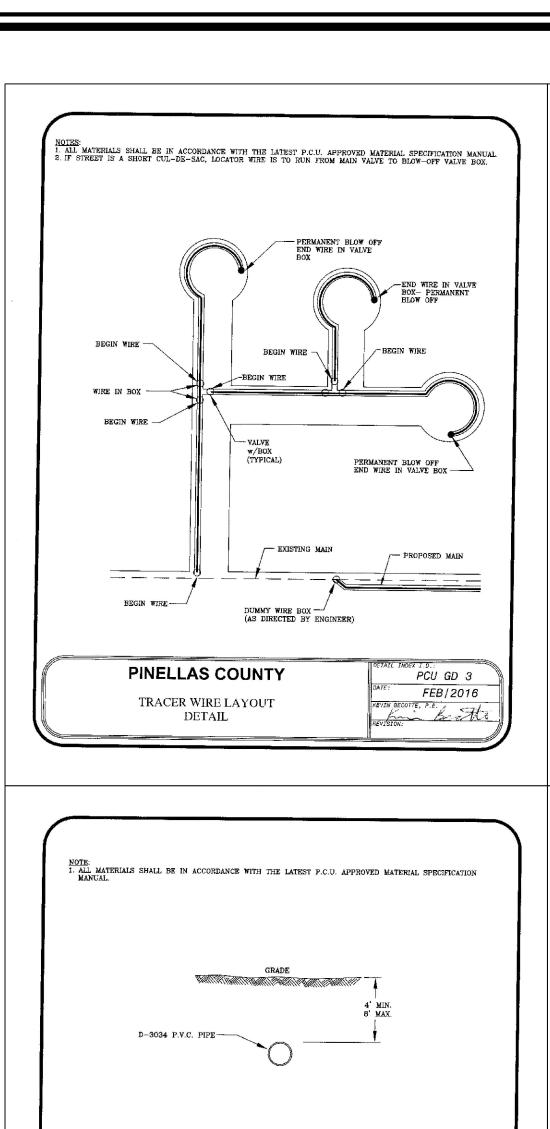
SILT FENCE AND EROSION CONTROL DETAIL

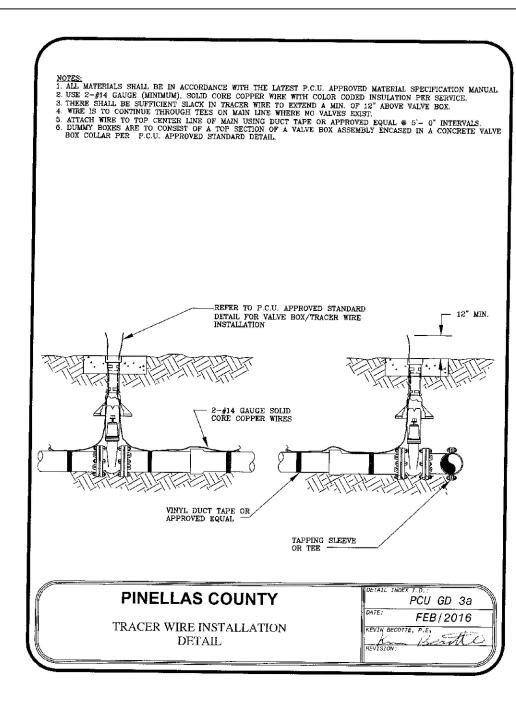
POST OPTIONS: WOOD 2-1/2" Ø MIN. WOOD 2"X4"

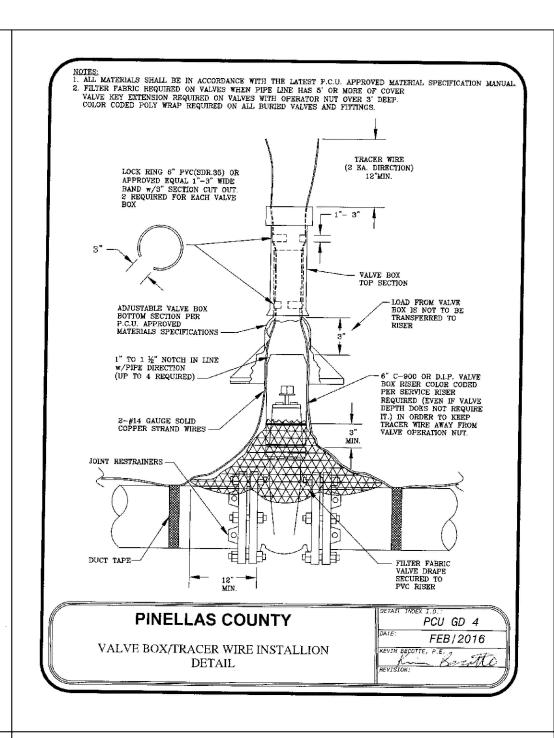
OAK 1-1/2"X1-1/2" STEEL1.33

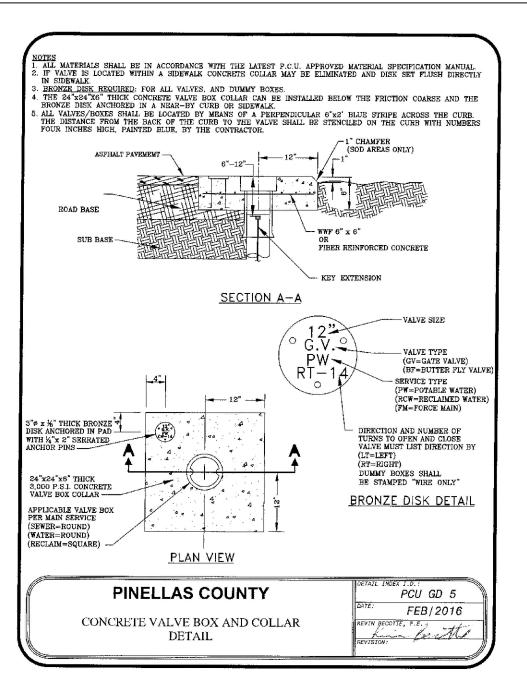


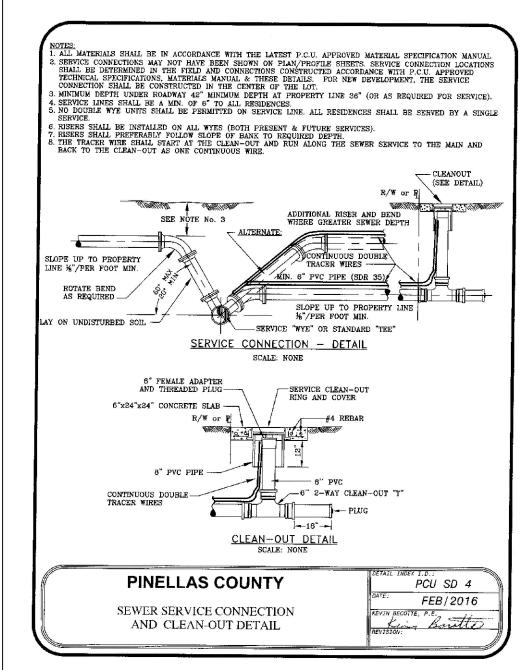
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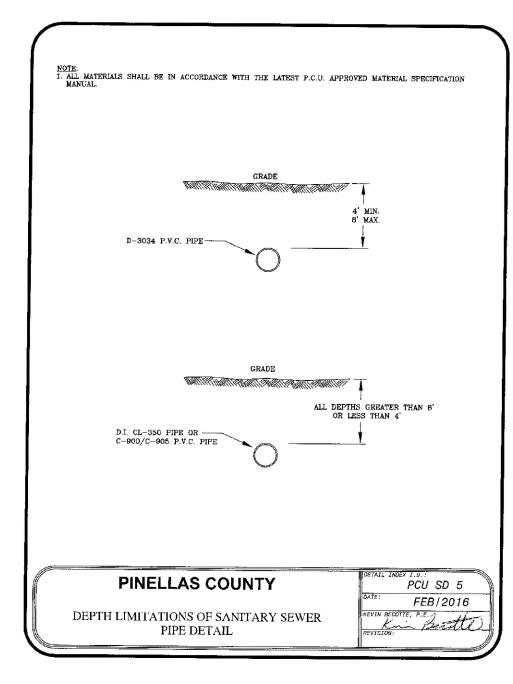


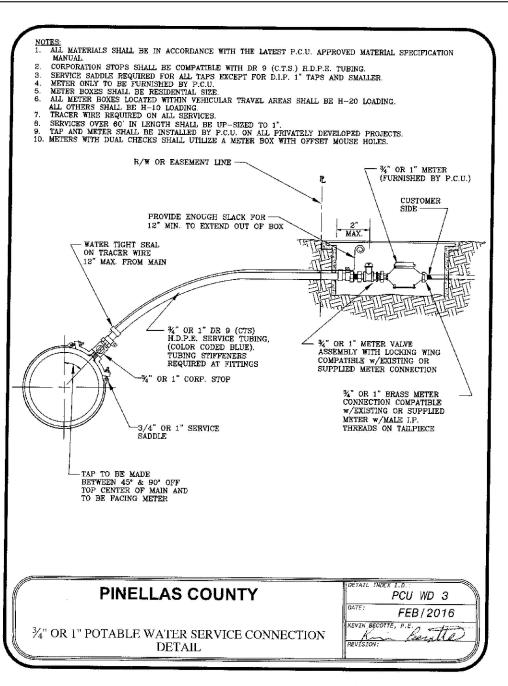


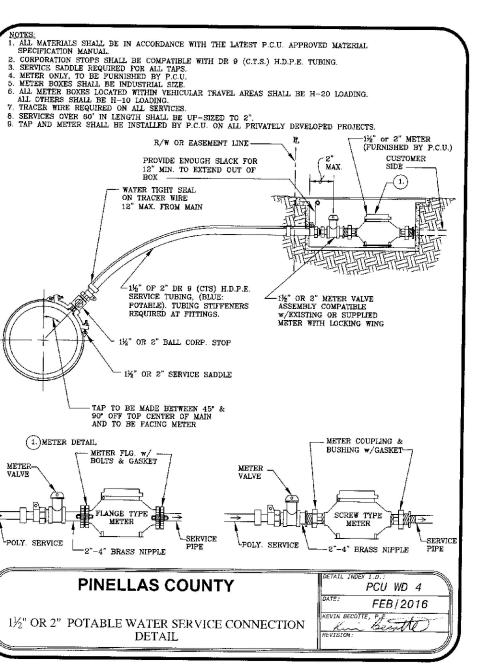


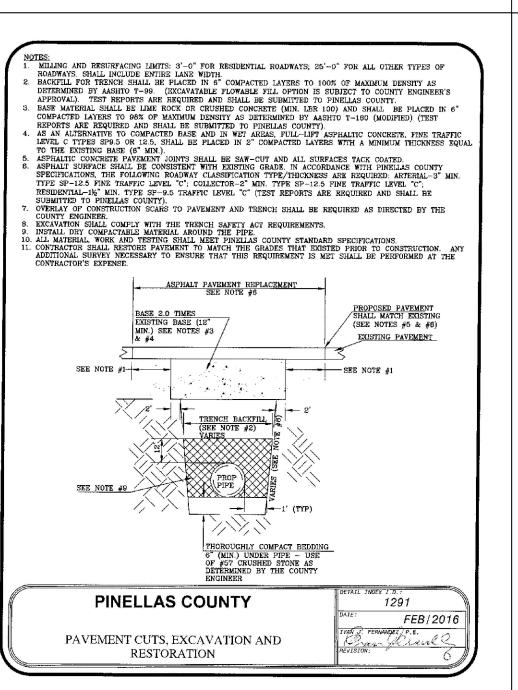


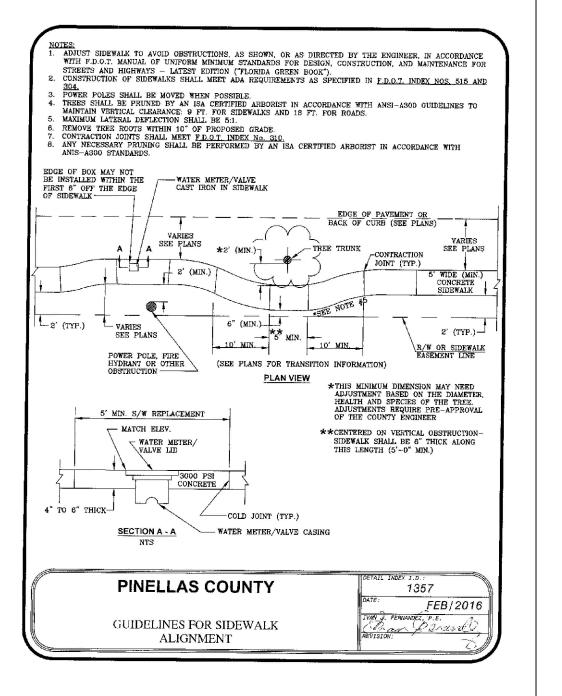


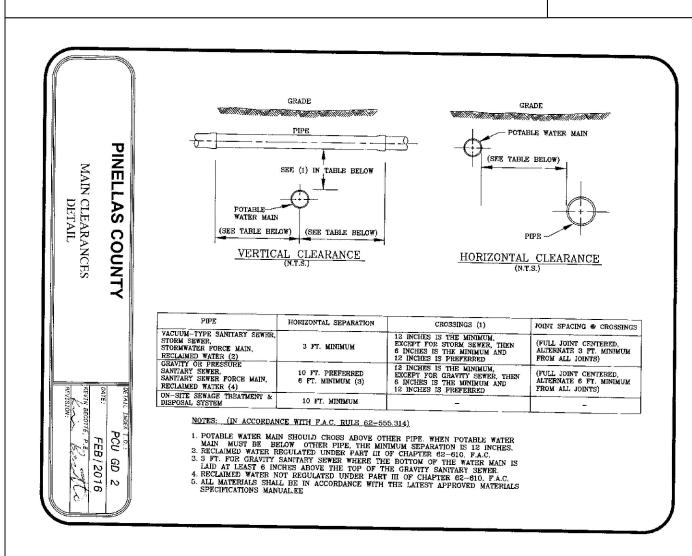


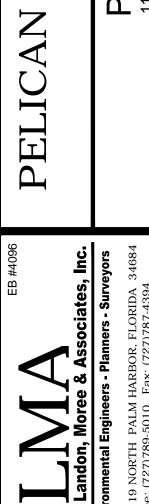












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**|**(**☎**).

ELICAN

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Civil & Environmental Engineers - Planners - Surveyors
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